

LONDON BOROUGH OF ENFIELD

AMERSHAM AVENUE N18, BULL LANE N18, SHAFTESBURY ROAD N18, TIVERTON ROAD N18, WILBURY WAY N18 - INTRODUCTION AND REMOVAL OF PEDESTRIAN CROSSINGS, INTRODUCTION OF MOTOR VEHICLE PROHIBITIONS AND AMENDMENTS TO PARKING PLACES AND WAITING RESTRICTIONS

Further information may be obtained from Traffic and Transportation, by emailing healthystreets@enfield.gov.uk

1. NOTICE IS HEREBY GIVEN that the Council of the London Borough of Enfield (the Council) propose to make The Enfield (Event Day) (Waiting Restrictions) (Amendment No. *) Order 202*, The Enfield (Waiting and Loading Restriction) (Amendment No. *) Traffic Order 202*, The Enfield (Event Day) (Parking Places) (Amendment No. *) Order 202*, The Enfield (Parking Places) (Pay and Display) (Various Roads, Upper Edmonton) (No. *) Order 202* and The Enfield (Prescribed Routes) (No. *) Traffic Order 202* under sections 6, 45, 46, 49 and 124 of and Part IV of Schedule 9 to the Road Traffic Regulation Act 1984, as amended.

2. The general effect of the Orders referred to in paragraph 1 of this Notice would be to:

- (a) increase the extent of the road closure area in Tiverton Road, at its junction with Bull Lane;
- (b) ban motor vehicles from entering or proceeding in Amersham Avenue N18, between the western kerb-line of Bull Lane and a point 12.5 metres west of that kerb-line;
- (c) ban the passage of motor vehicles in Shaftesbury Road N18, at a point at a point 3.7 metres east of the eastern kerb-line of Bull Lane;
- (d) ban vehicles, except buses and pedal cycles, from entering or proceeding in Bull Lane N18, between the northern kerb-line of gated entrance to Bull Lane Park and a point 8.8 metres north of that kerb-line
- (e) in Bull Lane N18:
 - (i) on the west side, opposite North Middlesex hospital car park, remove a pay and display parking place and replace it with double yellow line 'at any time' waiting restrictions;
 - (ii) on the east side, opposite the rear of Nos. 2, 2a, 4 and 4a Tiverton Road and Nos. 1 to 15 Giffard Road, relocate the event day 'SE' permit holders parking place onto the carriageway and reduce its length by 7 metres;
 - (iii) on the west side, at the rear of Nos. 2, 2a, 4 and 4a Tiverton Road and Nos. 1 to 15 Giffard Road, replace the existing event day single yellow line waiting restrictions with double yellow line 'at any time' waiting restrictions;
 - (iv) on the west side, at the side of No. 2 Amersham Avenue, provide 12.2 metres of event day 'SE' permit holders parking place;
 - (v) on the west side, remove the existing footway parking place between Shaftesbury Road the southern boundary of Bull Lane Park and replace it with double yellow line

'at any time' waiting restrictions and two event day 'SE' permit holders parking places (opposite No. 14 and No. 22);

- (f) in Amersham Avenue N18, remove the event day 'SE' permit holders parking place outside Nos. 1 and 3 and replace it with double yellow line 'at any time' waiting restrictions;
- (g) make minor amendments to the existing parking place definitions for Bull Lane so they better reflect how the scheme has been implemented on street (there would be no change to the way these parking places operate).

3. NOTICE IS ALSO HEREBY GIVEN that the Council propose to convert certain lengths of footway into cycle tracks, using powers under section 65 of the Highways Act 1980. Shared-use cycle tracks, for use by pedal cyclists and pedestrians, would be provided on the existing footway in Bull Lane.

4. FURTHER NOTICE IS HEREBY GIVEN that the Council of the London Borough of Enfield (the Council) has approved, under section 23 of the Road Traffic Regulation Act 1984:

- (a) the introduction of two new parallel (pedestrian and pedal cycle) crossings and one new zebra (pedestrian) crossings in Bull Lane N18 and Wilbury Way N18; and
- (b) the removal of the existing zebra (pedestrian) crossing from Bull Lane N18, just south of the access road to North Middlesex University Hospital.

The extent of the new crossings including the associated zig-zag markings (which would prohibit all vehicles from stopping on them for any reason at all times) would be as specified in the Schedule to this Notice.

5. A copy of the proposed Orders and of the Council's statement of reasons for proposing to make the Orders can be inspected at the Town Library, 66 Church Street, Enfield EN2 6AX, Monday to Fridays 9am - 5pm inclusive.

The notice, draft orders, and statement of reasons can also be inspected online at: <https://new.enfield.gov.uk/services/roads-and-transport/traffic-management-orders/>

Copies of the notice, draft orders and Statement of reasons can also be obtained by emailing healthystreets@enfield.gov.uk or by writing to Traffic and Transportation, Civic Centre, Silver Street, Enfield, Middlesex, EN1 3XD (quoting reference TG52/1483).

6. Persons desiring to object to the proposed Orders or make any other representations must do so by 27th October 2021. Any such objection or any representation must be in writing, quoting reference TG52/1483, and must state the grounds on which it is made. Objections or representations can be made in any of the following ways:

- online via the consultation survey on the project page at <http://letstalk.enfield.gov.uk/nmh-ati;>

- emailed to: healthystreets@enfield.gov.uk; or
- posted to: Healthy Streets Team, Enfield Council, Silver Street, Enfield, EN1 3XA.

Dated 6 October 2021

David B Taylor
Head of Traffic and Transportation

SCHEDULE

(Extent of the new parallel and zebra crossings including zig-zag markings)

Parallel Crossings - Bull Lane N18, from a point 10.4 metres north of the northern kerb-line of Watermill Lane northwards for a distance of 40.6 metres; and Wilbury Way N18, from a point 7.7 metres west of the western kerb-line of Bull Lane (measured on the southern side of Wilbury Way) westwards for a distance of 29.8 metres.

Zebra Crossing - Bull Lane N18, from a point 6.9 metres north of the northern kerb-line of Wilbury Way northwards for a distance of 27.3 metres.

AMERSHAM AVENUE N18, BULL LANE N18, SHAFTESBURY ROAD N18, TIVERTON ROAD N18, WILBURY WAY N18 - INTRODUCTION AND REMOVAL OF PEDESTRIAN CROSSINGS, INTRODUCTION OF MOTOR VEHICLE PROHIBITIONS AND AMENDMENTS TO PARKING PLACES AND WAITING RESTRICTIONS

The Enfield (Event Day) (Waiting Restrictions) (Amendment No. *) Order 202*, The Enfield (Waiting and Loading Restriction) (Amendment No. *) Traffic Order 2021, The Enfield (Event Day) (Parking Places) (Amendment No. *) Order 202*, The Enfield (Parking Places) (Pay and Display) (Various Roads, Upper Edmonton) (No. *) Order 202* and The Enfield (Prescribed Routes) (No. *) Traffic Order 202*

STATEMENTS OF REASONS

Enfield Council are proposing to implement an active travel route along Bull Lane N18.

The aim of this project is to provide a continuation of the recently implemented Cycleway 1 extension and a future connection with Cycle Superhighway 1 (CS1) in Haringey.

The total length of the proposed active travel route will be approximately 0.5km. It will extend from the A406 North Circular Road underpass at Bull Lane to the Enfield borough boundary at the southern end of Bull Lane.

The proposals feature a two-way segregated stepped cycle track on the northern part of Bull Lane (up to the junction with Wilbury Way), new crossings on Bull Lane and Wilbury Way for pedestrians and people who cycle, a bus gate at the southern end of Bull Lane, and modal filters at Amersham Avenue and Shaftesbury Road.

The proposed interventions will:

- Deliver a key active travel link which will provide improved access for key workers and visitors travelling to North Middlesex Hospital.
- Provide a quieter, safer, and more pleasant route that will encourage people to use active travel modes for more of their journeys.
- Reduce the volume of motor traffic on the part of Bull Lane south of its junction with Wilbury Way and Bridport Road, in order to encourage active travel. This can result in some motor vehicle drivers having to use different routes to access their destination.

Orders are being Made to support the above, in locations within the London Borough of Enfield.



THE COUNCIL OF THE LONDON BOROUGH OF ENFIELD TRAFFIC MANAGEMENT ORDER

202* No. *

The Enfield (Event Day) (Waiting Restrictions)
(Amendment No. *) Order 202*

Made ** ***** 2020

Coming into operation ** ***** 2020

The Council of the London Borough of Enfield, after consulting the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 6 and 124 of and Part IV of Schedule 9 to the Road Traffic Regulation Act 1984(a), and of all other powers thereunto enabling hereby make the following Order:-

1. This Order shall come into operation on **** 202* and may be cited as the Enfield (Event Day) (Waiting Restrictions) (Amendment No. *) Order 202*.

2. In this Order the expression "enactment" means any enactment, whether public general or local, and includes any order, bye-law, rule, regulation, scheme or other instrument having effect by virtue of an enactment and any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment.

(a) 1984 c.27

3. Without prejudice to the validity of anything done or to any liability incurred in respect of any act or omission before the coming into operation of this Order, The Enfield (Event Day) (Waiting Restrictions) Order 2018(b) shall have effect as though:

(i) for the item numbered 6 in Schedule 1 to that Order there were substituted the item similarly numbered and set out in columns 1, 2 and 3 of Schedule 1 to this Order;

(ii) the item numbered 15 in Schedule 1 to that Order were omitted; and

(iii) for the item numbered 8 in Schedule 2 to that Order there were substituted the item similarly numbered and set out in columns 1, 2 and 3 of Schedule 2 to this Order.

Dated this * day of * 202*

Head of Traffic and Transportation
(The officer appointed for this purpose)

(b) LBE 2018/47

SCHEDULE 1 (see Article 3)

1 Item	2 Length of street
6.	Amersham Avenue
	(a) The west side, from a point 7.5 metres north of the common boundary of Nos. 155 and 157 Amersham Avenue to a point 5 metres south of the common boundary of Nos. 151 and 153 Amersham Avenue.
	(b) The west side, from a point 6 metres north of the common boundary of Nos. 151 and 153 Amersham Avenue to a point 5 metres north-east of the common boundary of Nos. 147 and 149 Amersham Avenue.
	(c) The north side, from a point 1 metre east of the common boundary of Nos. 107 and 109 Amersham Avenue to a point 5.5 metres west of the eastern wall of No. 103 Amersham Avenue.
	(d) The north side, from a point 3 metres east of the eastern wall of No. 103 Amersham Avenue to a point 21 metres west of the western kerb-line of Weir Hall Road.
	(e) The north side, from a point 12 metres east of the eastern kerb-line of Weir Hall Road to a point 18.5 metres east of the eastern kerb-line of Weir Hall Road.
	(f) The north side, from a point 25.5 metres east of the eastern kerb-line of Weir Hall Road to a point 2 metres west of the western wall of No. 101 Amersham Avenue.
	(g) The north side, from a point in line of the western wall of No. 93 Amersham Avenue to a point 1.5 metres west of the eastern wall of No. 91 Amersham Avenue.
	(h) The north side, from a point 1.5 metres east of the common boundary of Nos. 87 and 89 Amersham Avenue to a point 1 metre west of the western wall of No. 85 Amersham Avenue.
	(i) The north side, from a point 0.5 metres west of the common boundary of Nos. 83 and 85 Amersham Avenue to a point 1 metre west of the western wall of No. 81 Amersham Avenue.
	(j) The north side, from a point in line of the common boundary of Nos. 79 and 81 Amersham Avenue to a point 1.5 metres west of the western wall of No. 77 Amersham Avenue.

SCHEDULE 1 (see Article 3)

1 Item	2 Length of street
	(k) The north side, from a point 1.5 metres east of the common boundary of Nos. 63 and 65 Amersham Avenue to a point 1 metre west of the western wall of No. 61 Amersham Avenue.
	(l) The north side, from a point in line of the western wall of No. 57 Amersham Avenue to a point 1 metre west of the eastern wall of No. 55 Amersham Avenue.
	(m) The north side, from a point 0.5 metres west of the common boundary of Nos. 51 and 53 Amersham Avenue to a point 0.5 metres west of the common boundary of Nos. 49 and 51 Amersham Avenue.
	(n) The north side, from a point 0.5 metres west of the common boundary of Nos. 47 and 49 Amersham Avenue to a point 6 metres west of the common boundary of Nos. 43 and 45 Amersham Avenue.
	(o) The north side, from a point 3 metres east of the common boundary of Nos. 43 and 45 Amersham Avenue to a point 4 metres west of the common boundary of Nos. 31 and 33 Amersham Avenue.
	(p) The north side, from a point 10.5 metres east of the common boundary of Nos. 31 and 33 Amersham Avenue to a point 14 metres west of the western kerb-line of Hardinge Road.
	(q) The north side, from a point 9 metres east of the eastern kerb-line of Hardinge Road to a point 6.5 metres west of the common boundary of Nos. 19 and 21 Amersham Avenue.
	(r) The north side, from a point 4 metres east of the common boundary of Nos. 13 and 15 Amersham Avenue to a point 15.5 metres west of the western kerb-line of Giffard Road.
	(s) The north side, from a point 13 metres east of the eastern kerb-line of Giffard Road to a point 7.1 metres west of the common boundary of Nos. 1 and 3 Amersham Avenue.
	(t) The east side, from a point 7 metres north of the common boundary of Nos. 146 and 148 Amersham Avenue to a point 17 metres south of the common boundary of Nos. 142 and 144 Amersham Avenue.
	(u) The south-east side, from a point 1 metre north of the common boundary of Nos. 142 and 144 Amersham Avenue to a point 6.5 metres south-west of the common boundary of Nos. 138 and 140 Amersham Avenue.

SCHEDULE 1 (see Article 3)

1 Item	2 Length of street
	(v) The south side, from a point 13 metres north-east of the north-eastern wall of No. 134 Amersham Avenue to a point 5.5 metres west of the common boundary of Nos. 130 and 132 Amersham Avenue.
	(w) The south side, from a point 6 metres east of the common boundary of Nos. 126 and 128 Amersham Avenue to a point 6.5 metres west of the common boundary of Nos. 114 and 116 Amersham Avenue.
	(x) The south side, from a point 5.5 metres east of the common boundary of Nos. 114 and 116 Amersham Avenue to a point 23 metres west of the western kerb-line of Weir Hall Road.
	(y) The south side, from a point 22.5 metres east of the eastern kerb-line of Weir Hall Road to a point 2 metres west of the western wall of No. 112 Amersham Avenue.
	(z) The south side, from a point 0.5 metres east of the western wall of No. 100 Amersham Avenue to a point in line of the common boundary of Nos. 98 and 100 Amersham Avenue.
	(aa) The south side, from a point in line of the eastern wall of No. 86 Amersham Avenue to a point 26 metres west of the western kerb-line of Barclay Road.
	(bb) The south side, from a point 31 metres east of the eastern kerb-line of Barclay Road to a point 0.5 metres east of the common boundary of Nos. 70 and 72 Amersham Avenue.
	(cc) The south side, from a point 0.5 metres east of the common boundary of Nos. 66 and 68 Amersham Avenue to a point 4 metres west of the common boundary of Nos. 62 and 64 Amersham Avenue.
	(dd) The south side, from a point 1 metre east of the common boundary of Nos. 62 and 64 Amersham Avenue to a point 0.5 metres east of the common boundary of Nos. 58 and 60 Amersham Avenue.
	(ee) The south side, from a point 4.5 metres east of the common boundary of Nos. 48 and 50 Amersham Avenue to a point in line of the common boundary of Nos. 40 and 42 Amersham Avenue.
	(ff) The south side, from a point 3.5 metres east of the eastern wall of No. 36 Amersham Avenue to a point 2 metres west of the common boundary of Nos. 32 and 34 Amersham Avenue.

SCHEDULE 1 (see Article 3)

1 Item	2 Length of street
	(gg) The south side, from a point 9.5 metres east of the common boundary of Nos. 32 and 34 Amersham Avenue to a point 4 metres east of the common boundary of Nos. 4 and 6 Amersham Avenue.

SCHEDULE 2 (see Article 3)

1 Item	2 Length of street
8.	Bull Lane
	1. The west side, from the northern kerb-line of the southernmost access to Bull Lane Park to a point 10.4 metres north of that kerb-line.
	2. The west side, from a point 27.2 metres north of the northern kerb-line of the southernmost access to Bull Lane Park to a point 104.1 metres north of that kerb-line.
	3. The east side, from a point opposite the northern kerb-line of the southernmost access to Bull Lane Park to a point 41.8 metres north of a point opposite that kerb-line.
	4. The east side, from a point 69.6 metres north of a point opposite the northern kerb-line of the southernmost access to Bull Lane Park to a point 43.8 metres south of the southern kerb-line of Shaftesbury Road

EXPLANATORY NOTE

(This Note is not part of the Order, but is intended to indicate its general purport).

This Order further amends The Enfield (Event Day) (Waiting Restrictions) Order 2018, so as to introduce "at any time" waiting restrictions in Bull Lane and Amersham Avenue, in support of additional provisions for cyclists, within the London Borough of Enfield.



THE COUNCIL OF THE LONDON BOROUGH OF ENFIELD TRAFFIC MANAGEMENT ORDER

202* No. *

The Enfield (Waiting and Loading Restriction)
(Amendment No. *) Order 2020

Made ** ***** 2020

Coming into operation ** ***** 2020

The Council of the London Borough of Enfield, after consulting the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 6 and 124 of and Part IV of Schedule 9 to the Road Traffic Regulation Act 1984(a), and of all other powers thereunto enabling hereby make the following Order:-

1. This Order shall come into operation on **** 202* and may be cited as the Enfield (Waiting and Loading Restriction) (Amendment No. *) Order 202*.

2. In this Order the expression "enactment" means any enactment, whether public general or local, and includes any order, bye-law, rule, regulation, scheme or other instrument having effect by virtue of an enactment and any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment.

(a) 1984 c.27

3. Without prejudice to the validity of anything done or to any liability incurred in respect of any act or omission before the coming into operation of this Order, the Enfield (Waiting and Loading Restriction) Order 2012(b) shall have effect as though for the items numbered 35 and 1593 in Schedule 1 to that Order there were substituted the item similarly numbered and set out in columns 1, 2 and 3 of the Schedule to this Order;

Dated this * day of * 202*.

Head of Traffic and Transportation
(The officer appointed for this purpose)

(b) LBE 2012/1

SCHEDULE (see Article 3)

1	2	3
<p>35.</p>	<p>Bull Lane</p> <p>(a) the east side</p> <p>(i) between a point 13 metres south of the southern kerb-line of Sterling Way and a point 42 metres south of the southern kerb-line of Bridport Road;</p> <p>(ii) between a point 29 metres north of a point opposite the northern kerb-line of Amersham Avenue and a point 7.4 metres south of the southern kerb-line of Shaftesbury Road.</p> <p>(b) the west side</p> <p>(i) between a point 13 metres south of the southern kerb-line of Sterling Way and a point 12 metres south of the southern kerb-line of Amersham Avenue;</p> <p>(ii) between a point and a point 24.2 metres south of the southern kerb-line of Amersham Avenue and a point 122.3 metres north of the northern kerb-line of the southernmost access to Bull Lane Park;</p>	<p>A</p> <p>A</p> <p>A</p> <p>A</p>
<p>1593.</p>	<p>Amersham Avenue</p> <p>(a) the north and north-west side</p> <p>(i) between a point 7.1 metres west of the common boundary of Nos. 1 and 3 Amersham Avenue and its junction with Bull Lane;</p> <p>(ii) between a point 10 metres west of the western kerb-line of Giffard Road and a point 10 metres east of the eastern kerb-line of Giffard Road;</p> <p>(iii) between a point 13.5 metres west of the western kerb-line of Hardinge Road and a point 10 metres east of the eastern kerb-line of Hardinge Road;</p> <p>(iv) between a point 12 metres west of the western kerb-line of Weir Hall Road and a point 12 metres east of the eastern kerb-line of Weir Hall Road;</p>	<p>A</p> <p>A</p> <p>A</p> <p>A</p>

SCHEDULE (see Article 3)

1	2	3
	<p>(v) between its junction with Weir Hall Avenue and a point 9.5 metres south of a point opposite the common boundary of No. 152 Amersham Avenue and No. 6 Weir Hall Avenue;</p> <p>(b) the south and south-east side</p> <p>(i) between the common boundary of Nos. 2 and 4 Amersham Avenue and its junction with Bull Lane;</p> <p>(ii) between a point 15.5 metres east of the western boundary of No. 84 Amersham Avenue and a point 10 metres west of the common boundary of Nos. 76 and 78 Amersham Avenue;</p> <p>(iii) between a point 12 metres west of the western kerb-line of Weir Hall Road and a point 12 metres east of the eastern kerb-line of Weir Hall Road;</p> <p>(iv) between its junction with Weir Hall Avenue and a point 9.5 metres south of a point opposite the common boundary of No. 152 Amersham Avenue and No. 6 Weir Hall Avenue.</p>	<p>A</p> <p>A</p> <p>A</p> <p>A</p> <p>A</p>

EXPLANATORY NOTE

(This Note is not part of the Order, but is intended to indicate its general purport).

This Order further amends the Enfield (Waiting and Loading Restriction) Order 2012, so as to introduce "at any time" waiting restrictions in Bull Lane and Amersham Avenue, in support of additional provisions for cyclists, within the London Borough of Enfield.



THE COUNCIL OF THE LONDON BOROUGH OF ENFIELD TRAFFIC MANAGEMENT ORDER

202* No. **

The Enfield (Event Day) (Parking Places)
(Amendment No. *) Order 202*

Made ** ***** 202*

Coming into operation ** ***** 202*

The Council of the London Borough of Enfield, after consulting the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 45, 46 and 49 and 124 of and Part IV of Schedule 9 to the Road Traffic Regulation Act 1984(a) as amended, and of all other powers thereunto enabling hereby make the following Order:-

Citation and commencement

1. This Order may be cited as the Enfield (Event Day) (Parking Places) (Amendment No *) Order 202* and shall come into operation on ** ***** 202*.

Interpretation

2. (1) In this Order, "the Order of 2018" means the Enfield (Event Day) (Parking Places) Order 2018(b);
(2) Any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment;
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(a) 1984 c.27 (b) L.B.E. 2018/46

- (3) Unless the context otherwise requires, any expression used in this Order which is also used in the Order of 2018 shall have the same meaning as in that Order.

Revocation of parking places

3. Without prejudice to the validity of anything done or to any liability incurred before the coming into operation of this Order, the Order of 2018 shall have effect as though the parking places numbered 172, 173, 174, 175, 176 and 177 in Schedule 1 to that Order were omitted.

Designation of parking places and application of the Order of 2018 thereto

4. (1) Each area on a highway comprising the length of carriageway of a street specified in column 2 of the Schedule to this Order and, bounded on one side of that length by the edge of the carriageway and having a width throughout of 1.8 metres is designated as a parking place.

(2) The provisions of the Order of 2018 (other than Articles 3 and 17) shall apply to the areas designated as parking places by this Order as if in those provisions any reference to a parking place included a reference to an area designated as a parking place by this Order, as if any reference to Schedule 1 to the Order of 2018 included a reference to the Schedule to this Order.

Placing of traffic signs, etc.

5. The Council shall:

- (a) place and maintain traffic signs indicating the limits of each parking place designated by this Order;
- (b) place and maintain in or in the vicinity of each parking place designated by this Order traffic signs indicating that such parking place may be used during the permitted hours for the leaving only of the vehicles specified in Article 5 of the Order of 2018;
- (c) carry out such other work as is reasonably required for the purposes of the satisfactory operation of a parking place.

Dated this ***** day of ***** 2020.

Head of Traffic and Transportation
(The officer appointed for this purpose)

SCHEDULE (see Article 4)

1 Item	2 Designated Parking Place	
*.	Bull Lane	the west side, from a point 12 metres south of the southern kerb-line of Amersham Avenue to a point 24.2 metres south of that kerb-line.
*.	Bull Lane	the west side, from a point 104.1 metres north of the northern kerb-line of the southernmost access to Bull Lane Park to a point 122.3 metres north of that kerb-line.
*.	Bull Lane	the west side, from a point 10.4 metres north of the northern kerb-line of the southernmost access to Bull Lane Park to a point 27.2 metres north of that kerb-line.
*.	Bull Lane	the east side, from a point 32.5 metres north of a point opposite the northern kerb-line of Amersham Avenue to a point 45.4 metres south of the southern kerb-line of Bridport Road.
*.	Bull Lane	the east side, from a point 7.4 metres south of the southern kerb-line of Shaftesbury Road to a point 43.8 metres south of that kerb-line.
*.	Bull Lane	the east side, from a point 41.8 metres north of a point opposite the northern kerb-line of the southernmost access to Bull Lane Park to a point 69.6 metres north of that kerb-line.

EXPLANATORY NOTE

(This Note is not part of the Order, but is intended to indicate its general purport).

This Order amends the extent of parking places in Bull Lane N18, in support of additional provisions for cyclists, within the London Borough of Enfield. The Order also amends the designation of certain other parking places in Bull Lane to correct historical drafting errors.



THE COUNCIL OF THE LONDON BOROUGH OF ENFIELD TRAFFIC MANAGEMENT ORDER

202* No. **

The Enfield (Parking Places) (Pay and Display) (Various Roads,
Upper Edmonton) (No. *) Order 202*

Made ** ***** 202*

Coming into operation ** ***** 202*

The Council of the London Borough of Enfield, after consulting the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 45, 46, 49, 51 and 124 of and Part IV of Schedule 9 to the Road Traffic Regulation Act 1984(a) as amended, and of all other powers thereunto enabling hereby make the following Order:-

Citation and commencement

1. This Order may be cited as The Enfield (Parking Places) (Pay and Display) (Various Roads, Upper Edmonton) (No. *) Order 202* and shall come into operation on ** ***** 202*.

Interpretation

2. (1) In this Order, "the Order of 2017" means the The Enfield (Parking Places) (Pay and Display) (Various Roads, Upper Edmonton) (No. 1) Order 2017(b);
 - (2) Any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment;
-

(a) 1984 c.27 (b) L.B.E. 2017/56

(3) Unless the context otherwise requires, any expression used in this Order which is also used in the Order of 2018 shall have the same meaning as in that Order.

Revocation of parking places

3. Without prejudice to the validity of anything done or to any liability incurred before the coming into operation of this Order, the Order of 2017 shall have effect as though the parking place numbered 4 in Schedule 1 to that Order were omitted.

Dated this ***** day of ***** 2020.

Head of Traffic and Transportation
(The officer appointed for this purpose

EXPLANATORY NOTE

(This Note is not part of the Order, but is intended to indicate its general purport).

This Order removes parking space in connection with the provision of cycle tracks and pedestrian crossings, in support of additional provisions for cyclists, within the London Borough of Enfield.



THE COUNCIL OF THE LONDON BOROUGH OF ENFIELD TRAFFIC MANAGEMENT ORDER

202* No. **

The Enfield (Prescribed Routes) (No. **) Traffic Order
202*

Made ** ***** 202*

Coming into operation ** ***** 202*

The Council of the London Borough of Enfield, after consulting the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 6 and 124 of, and Part IV of Schedule 9 to, the Road Traffic Regulation Act 1984(a) and of all other powers thereunto enabling, hereby make the following Order:-

Citation and commencement

1. This Order shall come into operation on ** ***** 202* and may be cited as the Enfield (Prescribed Routes) (No. **) Traffic Order 202*.

Interpretation

2. (1) In this Order:

"bus" and "pedal cycle" have the same meaning as in the Traffic Signs Regulations and General Directions 2016(b);

"cause" includes "permit"; and

"the Bull Lane bus gate" means that part of Bull Lane situated between the northern kerb-line of the southernmost gated entrance to Bull Lane Park and a point 8.8 metres north of that kerb-line.

(a) 1984 c.27 (b) S.I. 2016/362

- (2) Any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment.
- (3) Any reference in this Order to a numbered Article or Schedule shall, unless the context otherwise requires, be construed as a reference to the Article or Schedule bearing that number in this Order.

Prohibitions

3. No person shall cause any vehicle to enter or proceed in that part of Tiverton Road N18 that lies between the western kerb-line of Bull Lane and a point 1.9 metres north-east of a point in line with the north-eastern wall of No. 1a Tiverton Road.
4. No person causing any motor vehicle to proceed in a street specified in column (2) of the Schedule to this Order shall, upon reaching its junction with the street specified in relation thereto in column (3) of the Schedule, thereupon cause that vehicle to enter into that street specified in relation thereto in column (3) of that Schedule.
5. No person shall cause any motor vehicle to enter or proceed in Amersham Avenue N18, between the western kerb-line of Bull Lane N18 and a point 12.9 metres west of that kerb-line.
6. No person shall cause any vehicle other than a bus or pedal cycle to enter or proceed in the Bull Lane bus gate.

Exemptions

7. Nothing in Article 3 shall apply to any pedal cycle proceeding in a cycle track constructed in or by the side of the highway pursuant to Section 65 of the Highways Act 1980.
8. Nothing in Articles 5 or 6 of this Order shall apply:-
 - (a) to anything done with the permission of or at the direction of a police constable in uniform or a traffic warden; or

- (b) to any person who causes any vehicle to proceed in accordance with any restriction or requirement indicated by traffic signs placed pursuant to section 66 or section 67 of the Road Traffic Regulation Act 1984;
- (c) to any vehicle being used for ambulance, fire brigade or police purposes; or
- (d) in relation to any refuse collection vehicle, whilst being used for the purpose of the collection of refuse.

9. Without prejudice to the validity of anything done or to any liability incurred in respect of any act or omission before the coming into operation of this Order, The Enfield (Prescribed Routes) (No. 5) Traffic Order 2000(c) is hereby revoked.

Dated this ***** day of ***** 2020

David B. Taylor
Head of Traffic and Transportation

(c) LBE 2000/200

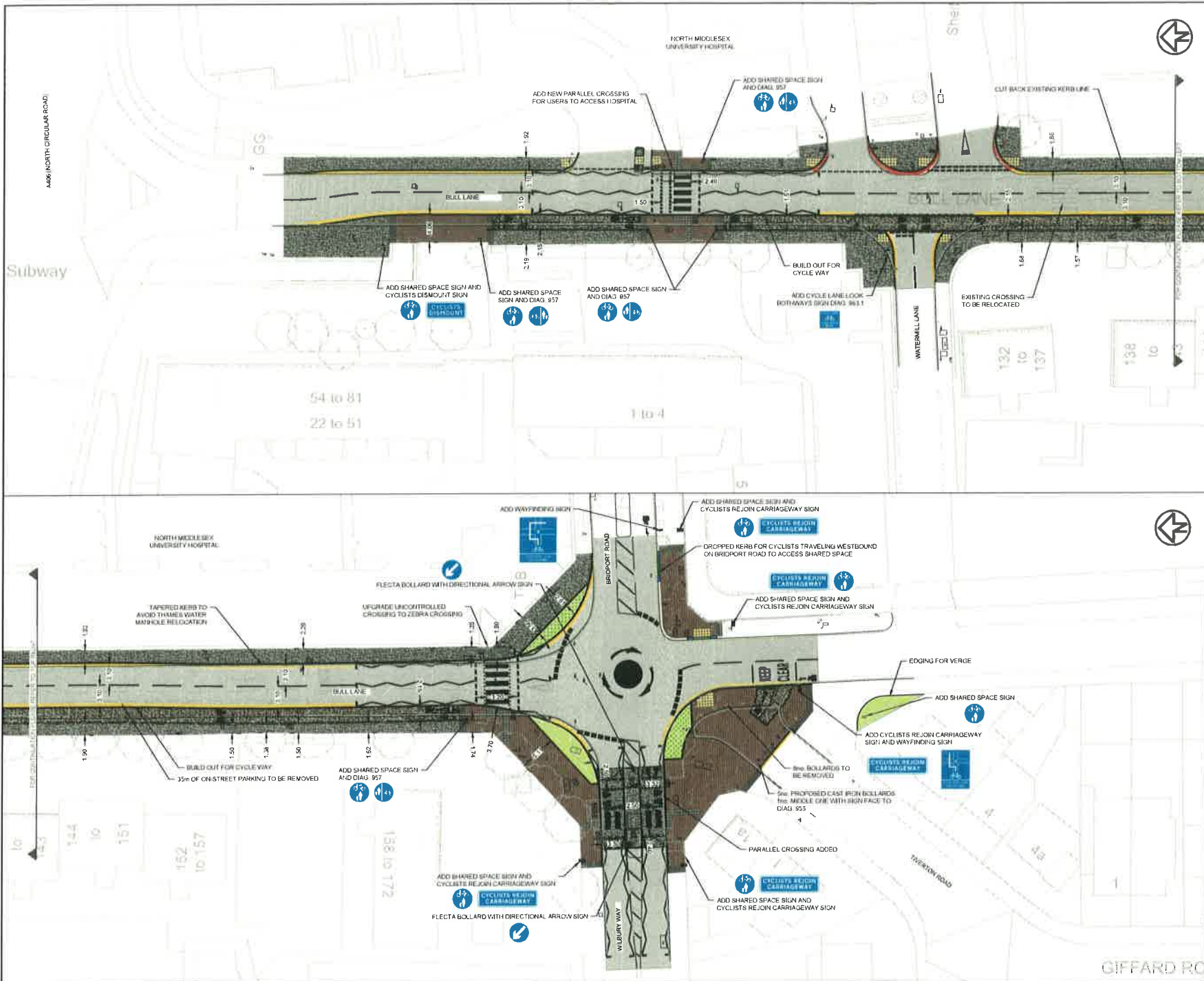
SCHEDULE (see Article 4)
POINTS SUBJECT TO A 'NO MOTOR VEHICLES' PROHIBITION

Item No. (1)	Street 1 (2)	Street 2 (3)
1.	SHAFTESBURY ROAD, east of a point 3.9 metres east of the eastern kerb-line of Bull Lane	SHAFTESBURY ROAD, west of a point 3.9 metres east of the eastern kerb-line of Bull Lane
2.	SHAFTESBURY ROAD, west of a point 3.9 metres east of the eastern kerb-line of Bull Lane	SHAFTESBURY ROAD, east of a point 3.9 metres east of the eastern kerb-line of Bull Lane

EXPLANATORY NOTE

(This Note is not part of the Order, but is intended to indicate its general purport).

This Order provides a road closure, bus gate and two no motor vehicle restrictions in support of additional provisions for cyclists, within the London Borough of Enfield.



- NOTES**
- THIS DRAWING SHALL BE READ IN CONJUNCTION WITH ALL METIS CONSULTANTS LTD DRAWINGS AND ALL OTHER RELEVANT ARCHITECTS, ENGINEERS AND LANDSCAPE ARCHITECTS DRAWINGS AND SPECIFICATIONS
 - ALL DIMENSIONS AND LEVELS SHOWN ARE IN METRES UNLESS OTHERWISE STATED
 - DO NOT SCALE FROM THIS DRAWING
 - LAYOUTS ARE A COMBINATION OF BOTH TOPOGRAPHICAL SURVEY AND ORDNANCE SURVEY CLARIFICATION AND DISCREPANCIES TO BE BROUGHT TO THE ATTENTION OF THE SITE ENGINEER
 - ALL ROAD MARKINGS AND SIGNS TO BE IN ACCORDANCE WITH THE TRAFFIC SIGNS REGULATION AND GENERAL DIRECTIONS (TSRGD) 2016 AND TRAFFIC SIGNS MANUAL (TSM) 2016
 - EXISTING 300 mm X 200 mm GRANITE KERBS TO BE REPLACED AS 200 mm X 300 mm

- LEGEND**
- CYCLE WAY - BUFF COLOUR FLEXIBLE SURFACING
 - FLEXIBLE SURFACING
 - CARRIAGEWAY RESURFACING
 - SHARED SPACE - HARVEST TEGULA BLOCKS
 - TRAMLINE ADORER TACTILE PAVING - CHARCOAL
 - RAIN GARDEN
 - GRASS VERGE
 - BLISTER TACTILE PAVING - CHARCOAL
 - BLISTER TACTILE PAVING - BUFF
 - WHITE RAISED THERMOPLASTIC MARKING
 - CONCRETE DEMARCATION BLOCK (W. 300 mm)
 - GRANITE KERB (W. 200 mm, H. 300 mm, U. VARIES)
 - GRANITE KERB (W. 150 mm, H. 150 mm, U. 50 mm)
 - PCC KERB (W. 125 mm, H. 100 mm)
 - FRANGIBLE KERBS
 - DROPPED KERB (W. 150 mm, H. 150 mm, U. 0 mm)
 - PROPOSED WHITE ROAD MARKINGS
 - PROPOSED YELLOW ROAD MARKINGS
 - PROPOSED RED ROAD MARKINGS
 - SIGN POST

REV	DATE	DESCRIPTION	DESIGN	CHKD	APPVD
1		PROPOSED AMENDMENTS FOR LANDSCAPE DESIGN REVIEW	GG	MP	AD
2		PROPOSED DETAILED DESIGN READY FOR CONSTRUCTION	GG	MP	AD
3		REVISIONS AMENDED TO AVOID THE MANHOLE	GG	MP	AD
4		DETAILED DESIGN READY 1	GG	MP	AD
5		FOR APPROVAL	GG	MP	AD
6		FOR INFORMATION	GG	MP	AD

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metisconsultants.co.uk

ENFIELD
Council

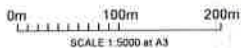
PROJECT
NORTH MIDDLESEX HOSPITAL
ACTIVE TRAVEL IMPROVEMENTS

DRAWING TITLE
GENERAL ARRANGEMENT

DRAWING STATUS
FOR INFORMATION

SCALE 1:200 @ A1 **DO NOT SCALE**
SHEET 001 OF 001
DRAWING NUMBER 20148-MET-HGN-DR-001 **REV** F

This drawing is not to be used in whole or part other than for the intended purpose and project as defined on this drawing. Refer to the contract for full terms and conditions.



Key:

- Existing Kerblino
- Proposed cycleway
- Drawing number and reference
- Proposed filter or jack gate

- General Notes**
1. All dimensions in metres, unless otherwise noted.
 2. Detail scale of plan.
 3. Dimensions are to be provided by the contractor prior to commencement of work. An allowance should be made for construction tolerances.
 4. Drawing based on OLE data.
 5. Co-ordinates based on OLE grid.
 6. Levels based on OLE datum (height).
 7. The drawing is to read in conjunction with all other relevant drawings and specifications.
 8. All work shall be carried out in accordance with L32 British Signage Authority and Health & Safety requirements and regulations.
 9. The position of vehicles is based on information provided by other parties at the time of issue and is for guidance only. It is the responsibility of the Client and Contractor to verify the exact position of any services before commencing works on site.
 10. Temporary traffic works must be undertaken in accordance with Chapter 8 of the Traffic Signs Manual, Signs Manual, Section 610 and 611 and Road Works Manual, Signs Manual, Section 610 and 611.
 11. These drawings have been produced under the CDM 2015 Regulations. The Client is deemed to have accepted the design and construction of the works.
 12. The liability of this drawing in absolute terms shall not be used to justify any claim or right in the event of any other party to any claim after or otherwise on the information provided. Any use of the information in this way is at the risk and liability of the user and the user accepts no liability for misinterpretation or alteration of the information contained herein.
 13. To ensure the road is safe to use during the period the proposed drawing is in use, the road should be closed to traffic.
 14. For further information on drawing and design revisions, see the revision log or contact the design team.
 15. This drawing has been produced for use as a guide. It is the user's responsibility to ensure that the drawing is used in the correct context and that any other relevant information is taken into account.

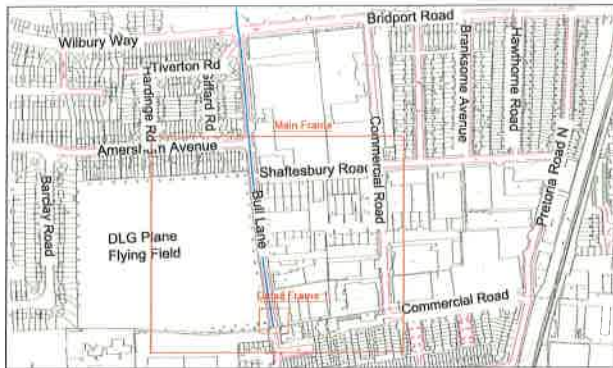
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Project: **NMH Active Travel Improvements**

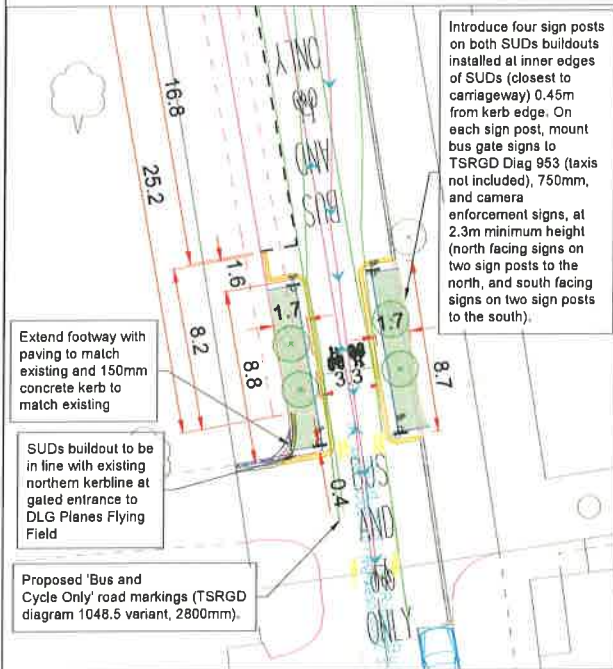
Title: **Overview Plan**

Drawn: Check: Appr: Date: Scale at A3:
 ZH OG OG 23/08/2021 1:5000

Status: **CONCEPT**
 Drawing No: **NMH-ATI-C-OP-01-00-01** Revision:



LOCALITY PLAN



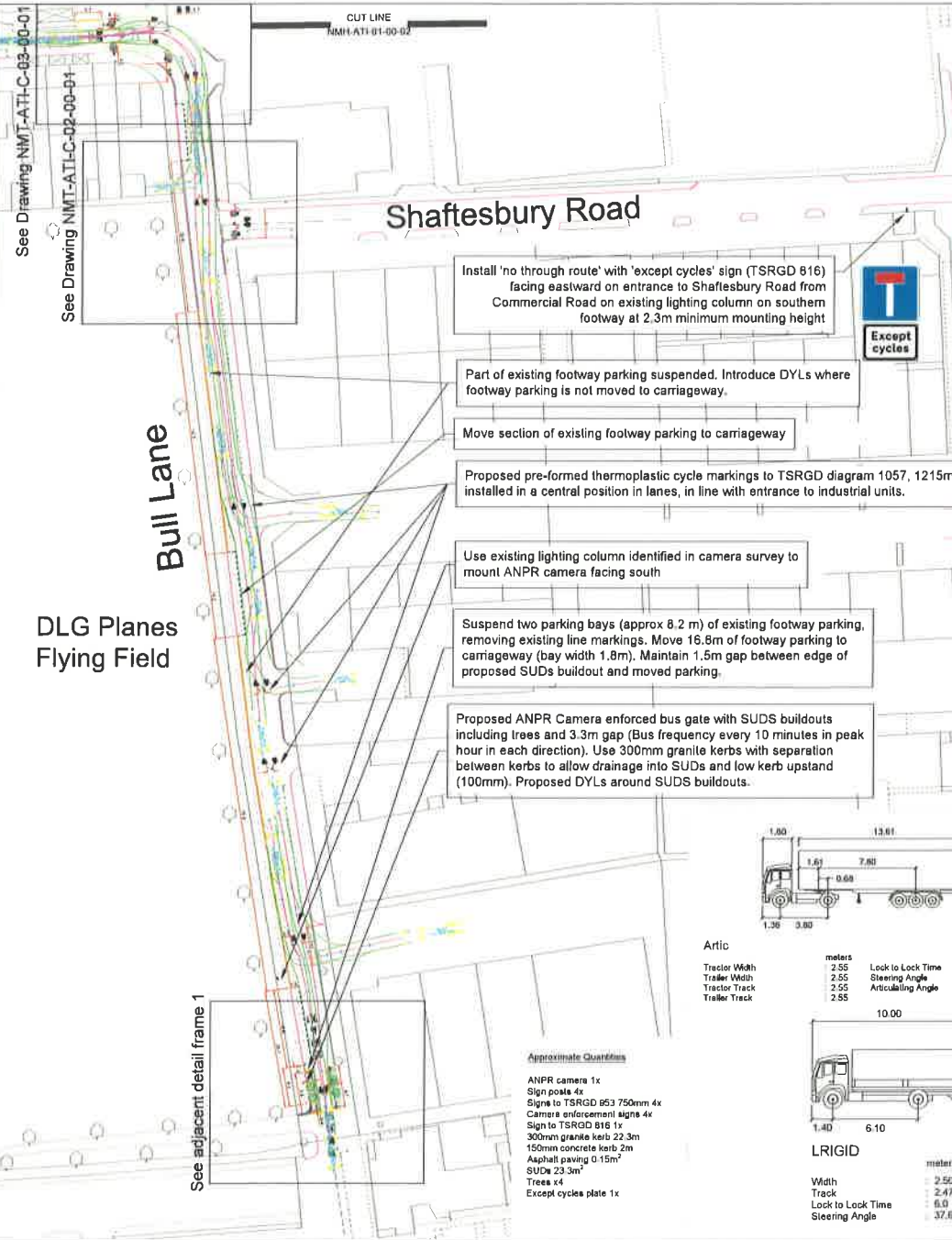
Detail Frame 1

Extend footway with paving to match existing and 150mm concrete kerb to match existing

SUDs buildout to be in line with existing northern kerblines at gated entrance to DLG Planes Flying Field

Proposed 'Bus and Cycle Only' road markings (TSRGD diagram 1048.5 variant, 2800mm).

Introduce four sign posts on both SUDs buildouts installed at inner edges of SUDs (closest to carriageway) 0.45m from kerb edge. On each sign post, mount bus gate signs to TSRGD Diag 953 (taxis not included), 750mm, and camera enforcement signs, at 2.3m minimum height (north facing signs on two sign posts to the north, and south facing signs on two sign posts to the south).



See Drawing NMT-ATI-C-03-00-01

See Drawing NMT-ATI-C-02-00-01

Bull Lane

DLG Planes Flying Field

See adjacent detail frame 1

Shafesbury Road

Install 'no through route' with 'except cycles' sign (TSRGD 816) facing eastward on entrance to Shafesbury Road from Commercial Road on existing lighting column on southern footway at 2.3m minimum mounting height



Part of existing footway parking suspended. Introduce DYLS where footway parking is not moved to carriageway.

Move section of existing footway parking to carriageway

Proposed pre-formed thermoplastic cycle markings to TSRGD diagram 1057, 1215mm, installed in a central position in lanes, in line with entrance to industrial units.

Use existing lighting column identified in camera survey to mount ANPR camera facing south

Suspend two parking bays (approx 8.2 m) of existing footway parking, removing existing line markings. Move 16.8m of footway parking to carriageway (bay width 1.8m). Maintain 1.5m gap between edge of proposed SUDs buildout and moved parking.

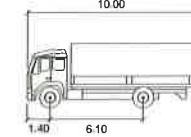
Proposed ANPR Camera enforced bus gate with SUDs buildouts including trees and 3.3m gap (Bus frequency every 10 minutes in peak hour in each direction). Use 300mm granite kerbs with separation between kerbs to allow drainage into SUDs and low kerb upstand (100mm). Proposed DYLS around SUDs buildouts.

Approximate Quantities

- ANPR camera 1x
- Sign posts 4x
- Signs to TSRGD 953 750mm 4x
- Camera enforcement signs 4x
- Sign to TSRGD 816 1x
- 300mm granite kerb 23.3m
- 150mm concrete kerb 2m
- Asphalt paving 0.15m²
- SUDs 23.3m²
- Trees x4
- Except cycles plate 1x

Artic

Tractor Width	2.55	Lock to Lock Time	6.0
Trailer Width	2.55	Steering Angle	42.7
Tractor Track	2.55	Articulating Angle	70.0
Trailer Track	2.55		



LRIGID

Width	2.50
Track	2.47
Lock to Lock Time	6.0
Steering Angle	37.6

Commercial Road

Key:

- Existing kerb line (OG base)
- Proposed cycleway route
- Existing lighting column
- Proposed yellow line marking
- Existing line marking
- Existing sign post
- Existing tree
- Existing sign post with proposed sign
- Proposed collapsible bollard with reflectors
- Proposed white line marking
- Proposed rectangular wooden planter 1.5m L x 0.9m W x 0.92m H with reflectors
- Proposed rectangular boll down kerb - 0.914m L x 0.15m W x 0.1m H (Redwood Harlow Kerb)
- Proposed rectangular wooden planter 0.9m L x 0.9m W x 0.92m H with reflectors
- Proposed line marking removal
- Proposed ANPR Camera
- SUDs
- Proposed 300mm granite kerb
- Proposed 150mm concrete kerb
- Proposed paving to match existing footway
- Proposed tree
- Proposed 150mm flush concrete kerb

- Notes**
- Part of drawing based on OS base, dimensions and asset furniture positions to be checked on site and any discrepancies to be reported.
 - All cycle markings to be pre-formed thermoplastic.
 - All planers and bollards to be equipped with reflectors.
 - Allow small gap between kerb and planer for drainage.
 - Use boll down kerbs to fit planers on carriageway in place. Allow gaps between kerbs, and gaps between boll down kerbs and kerbs for drainage. Boll down kerbs sections to be saw cut on site to required lengths.
 - Signs to be mounted with anti-rattle brags.
 - Total length of parking bays to be suspended 131.0m approx.
 - Proposed parking bay width in carriageway to be 1.8m.
 - Sowed path analysis, using large articulated vehicle 16.5m tracked at entrance to main industrial units. 15m rigid vehicles tracked where industrial units do not accommodate large articulated vehicles. Dry steering used.
- General Notes**
- All dimensions to be in metres, unless otherwise noted.
 - Do not scale off plan.
 - Consult with the owner of the site prior to construction of work. Any discrepancy shall be reported immediately to the client.
 - Construction based on OS base data.
 - Construction based on OS and ground conditions.
 - The drawing is to be read in conjunction with all relevant drawings and specifications.
 - All work shall be carried out in accordance with UK Building Regulations and all health and safety requirements and regulations.
 - The position of services is based on information provided by other parties at the time of design and is for guidance only. It is the responsibility of the contractor to verify the exact position of any services before commencing work on site.
 - Temporary works shall be undertaken in accordance with Chapter 8 parts 1 & 2 of the Traffic Signs Manual, Safety at Street Works and Road Works Code of Practice 1997 and the Regulations of CDM 2015.
 - These drawings have been produced under the CDM 2015 Regulations. The client is responsible for ensuring that the contractor is fully aware of the duty of the designer or architect to ensure that the contractor is fully aware of the risks and the consequences of any work on site. The contractor is responsible for ensuring that the contractor is fully aware of the risks and the consequences of any work on site.
 - To ensure the most up to date drawings are being used the project drawing register should be checked and design changes, via a design log or contact the design team manager.
 - The drawing has been produced for the use of the client and is not to be used for any other purpose. It is the responsibility of the contractor to ensure that the drawing is used for the intended purpose and is not used for any other purpose. It is the responsibility of the contractor to ensure that the drawing is used for the intended purpose and is not used for any other purpose.

A | Enrich Design Review | ZH | DD | GG | 05/08/2021

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Project: NMH Active Travel Improvements

Title: Bull Lane South (1) General Arrangement

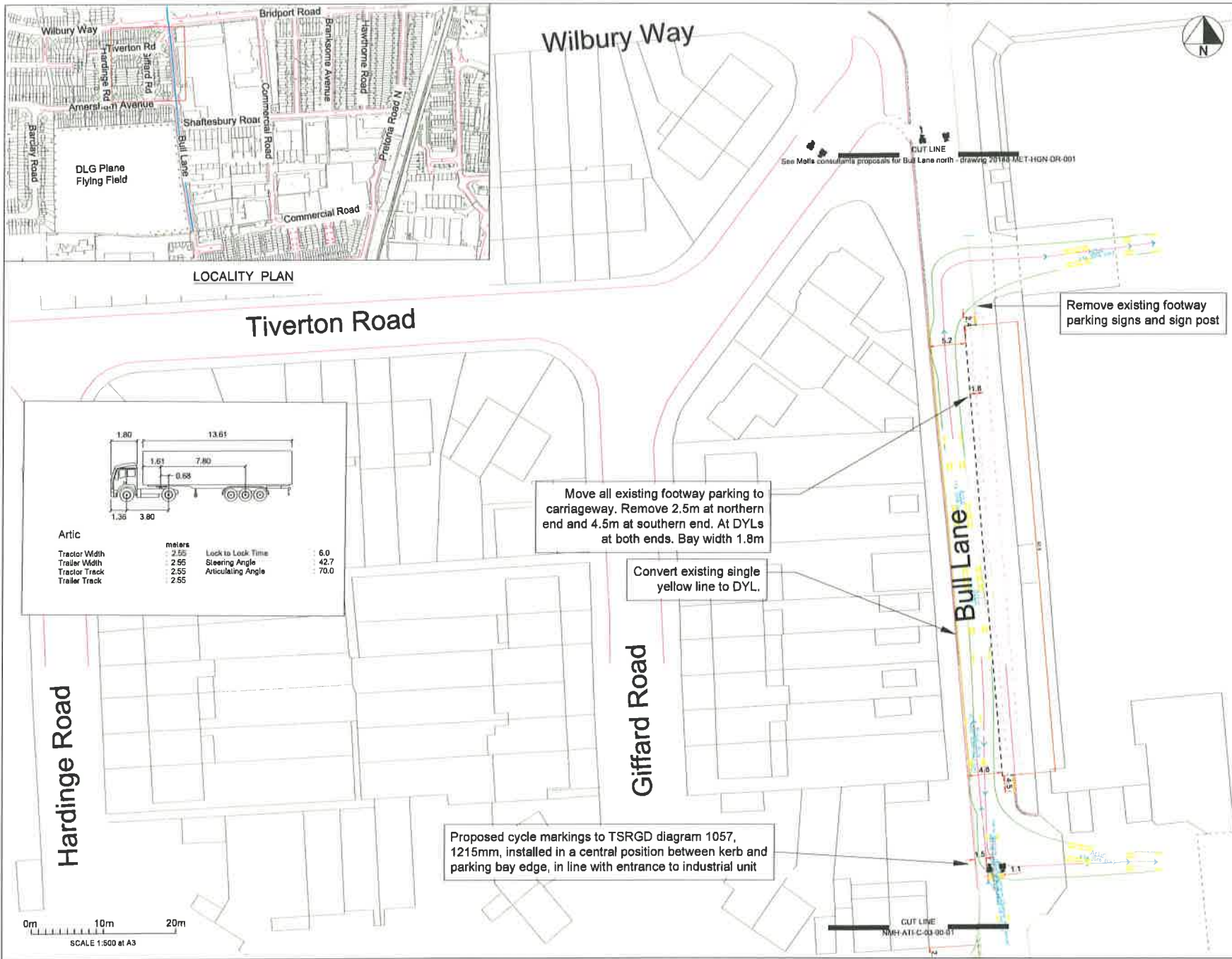
Drawn: ZH | Check: JAG | Appr: JAG | Date: 20/05/2021 | Scale at A3: 1:1000

Status: **CONCEPT**

Drawing No: NMH-ATI-C-01-00-01 | Revision: A



SCALE 1:1000 at A3



Key:

- Existing kerb line (OG base)
- Proposed cycleway route
- Existing lighting column
- Proposed yellow line marking
- Existing line marking
- Existing sign post
- Remove existing sign post with signs
- Proposed white line marking
- Proposed line marking removal

Notes:

- Part of drawing based on OS base, dimensions and street furniture positions to be checked on site and any discrepancies to be reported.
- All cycle markings to be pre-formed thermoplastic.
- Street path analysis using large articulated vehicle 18.5m length at entrance to main industrial unit.
- Proposed carriageway parking width 1.8m.

General Notes:

- All dimensions in metric, unless otherwise stated.
- Drawn to site of plan.
- Dimensions to be checked by the contractor prior to commencement of work. Any discrepancies are to be reported immediately to the client.
- Drawing based on OS base data.
- Construction based on OS plan.
- Levels based on OS datum (AOD).
- This drawing is to be read in conjunction with all other relevant drawings and specifications.
- All work shall be carried out in accordance with the contract and any relevant standards and specifications.
- Contractor to ensure that all work is carried out in accordance with the contract and any relevant standards and specifications.
- The position of services is shown as indicated on the drawing and is for guidance only. It is the responsibility of the client and contractor to verify the exact location of any services before commencing work on site.
- Temporary traffic signs must be provided in accordance with Chapter 8 parts 1 & 2 of the Traffic Signs Manual, Subject to Street Works and Road Works Code of Practice 2018 and any other relevant legislation.
- Final drawings have been produced under the CDM 2015 Regulations. The client is advised to refer to the Regulations of CDM 2015.
- The absence of this drawing in electronic format shall not be construed to indicate any authorisation or right of the contractor or any other person to rely upon, alter or otherwise use the information provided. Any use of this information on this site and liability of the user and their own contractors shall be the responsibility of the user and their own contractors.
- In the event the contractor does not agree with the proposed drawing, the contractor should be referred to the client.
- For further information on drawing and design standards, see drawing log in Contract Documents project folder.
- The drawing has been produced to read in colour, for the most site specific information. Printing in black and white, or on a different scale may lead to misinterpretation of the drawing.

Approval: A | Eirfield Design Review | ZH | 00 | 00 | 05/08/2021

Logo: **sustrans** JOIN THE MOVEMENT

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Project: NMH Active Travel Improvements

Title: Bull Lane South (2) General Arrangement

Drawn: ZH | **Check:** JAG | **Appr:** JAG | **Date:** 24/05/2021 | **Scale at A3:** 1:500

Status: **CONCEPT**

Drawing No: NMH-ATI-C-01-00-02 | **Revision:** A



LOCALITY PLAN

Proposed on-carriageway parking bays to partially offset parking loss further south



Proposed pre-formed thermoplastic cycle markings to TSRGD diagram 1057, 1215mm, installed in a central position in lanes

Proposed filter with one 0.9 m L x 0.9m W x 0.92m H rectangular planter and one 1.5m x 0.9m x 0.92m H rectangular planter, both equipped with reflectors, with collapsible bollard in between (also equipped with reflectors). Planters installed in line with wooden fence edge on northern footway of Shaftesbury Road. Use four bolt down kerb sections around perimeter of each planter to fix planters in place. Maintaining a gap of 0.15m from kerbside.

Proposed pre-formed thermoplastic cycle markings to TSRGD diagram 1057, 1215mm, installed in a central position between kerb and parking bay edge, in line with start of parking bays.

Install 2 new signposts adjacent to proposed planters (east side), each with 2x 'motor vehicles prohibited' signs to TSRGD diagram 619, 750 mm mounted east and west facing on the proposed signposts at 2.3m minimum mounting height.



- Key:
- Existing kerb line (OG base)
 - Proposed cycleway route
 - Existing lighting column
 - Proposed yellow line marking
 - Existing line marking
 - Existing sign post
 - Existing tree
 - Existing sign post with proposed sign
 - Proposed collapsible bollard with reflectors
 - Proposed white line marking
 - Proposed rectangular wooden planter 1.5m L x 0.9m W x 0.92m H with reflectors
 - Proposed rectangular boll down kerbs - 0.914m L x 0.15m W x 0.1m H (Radwek Hallow Kerbs)
 - Proposed rectangular wooden planter 0.9m L x 0.9m W x 0.92m H with reflectors
 - Proposed line marking removal

- Approximate Quantities
- Collapsible bollard with reflectors - 1x
 - Planter with reflectors 1.5m x 0.9m x 0.92m - 1x
 - Planter with reflectors 0.9m x 0.9m x 0.92m - 1x
 - Boll down Kerbs - 8x
 - Signposts - 2x
 - Signs to TSRGD 619, 750mm - 4x
- Notes:
1. Part of drawing based on OG base, dimensions and street furniture positions to be checked on site and any discrepancies to be reported.
 2. All cycle markings to be pre-formed thermoplastic.
 3. All planters and bollards to be equipped with reflectors.
 4. Allow small gap between kerb and planter for drainage.
 5. Use boll down kerbs to fix planters in carriageway in place. Allow gaps between kerbs, and gaps between boll down kerbs and kerbside for drainage. Boll down kerbs sections to be saw cut on site to required lengths.
 6. Signs to be mounted with anti-rotation fixings.
 7. Total length of parking bays to be suspended along Bull Lane (31.0m approx (including gain of relocated parking)).
 8. Proposed parking bay width in carriageway to be 1.6m.
 9. Sheep pain analysis. Large suburban SUV treated at driveway at 40km/h. No sky steering used.

- General Notes:
1. All dimensions in metres, unless otherwise stated.
 2. Do not scale off plan.
 3. Drawings shall be checked by the contractor prior to commencement of work. Any discrepancies shall be reported immediately to the client.
 4. Drawing based on OG base plan.
 5. Coordinate based on OS.
 6. Levels based on OS datum (Mean Sea Level).
 7. This drawing is to be read in conjunction with all other relevant drawings and specifications.
 8. All work shall be carried out in accordance with BS 5393 (British Safety Authority) and health & safety requirements and regulations.
 9. The position of proposed work is based on information provided by other parties on the time of design and is for guidance only. It is the responsibility of the client and contractor to verify the exact position of any services before commencing work on site.
 10. Traffic signs shall be marked with the undertaking in accordance with Chapter 8 of the Traffic Signs Manual (TSM) and any other relevant legislation.
 11. The design of the proposed work shall be undertaken in accordance with the Traffic Signs Manual (TSM) and any other relevant legislation.
 12. The client is advised to check the design of the proposed work with the relevant authority or other parties prior to construction to ensure that the design meets the requirements of the relevant authority and any other parties.
 13. To ensure the most up to date drawings are being used the client drawing register should be referred to.
 14. For further information on drawings and design revisions, see drawing log or contact the design team.
 15. The drawing has been produced for the client's use. It is the client's responsibility to ensure that the drawing is used for the intended purpose and is not altered without the client's consent.

A | ERMAD Design Review | ZH | 06 | 06 | 05/08/2021

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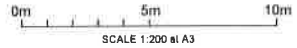
Project: NMH Active Travel Improvements

Title: Shaftesbury Road Filter
General Arrangement

Drawn: ZH | Check: JAG | Appr: JAG | Date: 20/05/2021 | Scale at A3: 1:200

Status: **CONCEPT**

Drawing No: NMH-AT1-C-02-00-01 | Revision: A





LOCALITY PLAN

Introduce 2x sign posts with 'motor vehicles prohibited' sign to TSRGD diagram 619, 750 mm on each sign post mounted at 2.3m minimum mounting height. Sign posts to be installed behind planters (inside the closure), in line with centre of planters, with signs facing the road outside the closure (eastward)



Existing dropped kerb being used as crossover. Vehicles no longer able to legally access space with presence of filter.



Remove sign post and signs related to 20mph zone (east facing) and 30mph zone (west facing).

Install 'motor vehicles prohibited' sign to TSRGD diagram 619, 750 mm mounted west facing on existing signpost at 2.3m minimum mounting height.

Suspend one parking bay and introduce DYLS instead.

Amersham Avenue

Extend existing parking sign post and introduce 'no through route' sign to TSRGD 816, with 'except cycles' plate mounted facing westwards at minimum 2.3m mounting height.



Proposed filter with two rectangular planters equipped with reflectors, installed in line with existing signposts on footway. Maintain access to driveway on southern side of Amersham Avenue. Use four bolt down kerb sections around perimeter of each planter to fix planters in place. Maintaining a gap of 0.2m from kerbside.

Two proposed rectangular planters, 11.4m apart equipped with reflectors installed in line with existing give way line. Use four bolt down kerb sections around perimeter of each planter to fix planters in place.

Proposed pre-formed thermoplastic cycle markings to TSRGD diagram 1057, 1215mm, installed in a central position in lanes

Remove signs related to vehicle weight restrictions (east facing) and end of weight restriction zone (west facing).

Disconnect existing lit signpost. Replace with 75mm signpost (moved from existing 20mph sign) to install 'motor vehicles prohibited' sign to TSRGD diagram 619, 750 mm mounted west facing at 2.3m minimum mounting height.

Remove signs related to 20mph zone (east facing) and 30mph zone (west facing). Remove and relocate straight sign post to location adjacent to planter to mount sign to TSRGD diagram 619.



SCALE 1:200 at A3

CUT LINE
NMH-ATI-C-02-00-01



Key:

- Proposed driveway
- Existing kerb line (OS base)
- Proposed cycleway route
- Existing lighting column
- Proposed yellow line marking
- Existing line marking
- Existing sign post with sign
- Existing tree
- Existing sign post with proposed sign
- Proposed collapsible bollard with reflectors
- Proposed white line marking
- Proposed rectangular wooden planter 1.5m L x 0.8m W x 0.92m H with reflectors
- Proposed rectangular bolt down kerb - 0.814m L x 0.15m W x 0.1m H (Redweld Hallow Kerbs)
- Existing sign post and sign to be removed
- Proposed line marking removal
- Alternative filter design - proposed bollard with dimensions
- New/relocated sign post with proposed sign

Approximate Quantities

- Planter with reflectors - 15 m x 0.8m x 0.92m - 4x
- Bolt Down Kerb - 16x
- Signs to TSRGD 619 - 750mm - 4x
- Sign to TSRGD 816 - 1x
- Control cycle plate - 1x
- Sign posts - 2x

- Notes:**
1. Part of drawing based on OS data, dimensions and street furniture positions to be checked on site and any discrepancies to be reported.
 2. Proposed cycle markings to be pre-formed thermoplastic.
 3. All planters and bollards to be equipped with reflectors.
 4. Allow small gap between kerbs and planter for drainage.
 5. Use bolt down kerbs for planters in carriageway if placed. Allow gaps between kerbs, and gaps between bolt down kerbs and kerbside for drainage. Bolt down kerbs sections to be saw cut on site to required lengths.
 6. Low planters to be used in planters to avoid obstructing visibility.
 7. Signs to be mounted with anti-rattleback fixings.
 8. Planting suspended on this location is 2.7m.
 9. Proposed parking bay width in carriageway to be 1.4m.

- General Notes:**
1. All dimensions in metric, unless otherwise noted.
 2. Council work off plan.
 3. Dimensions are to be checked by the contractor prior to commencement of work. Any discrepancies shall be reported immediately to Sustrans.
 4. Drawing based on OS base data.
 5. Coordinates based on OS.
 6. Levels based on OS datum (Height).
 7. All work shall be carried out in accordance with all relevant drainage and floodplain.
 8. All work shall be carried out in accordance with 1.8 Code of Practice for Traffic Signs, National Safety Council Works and Road Works Code of Practice 2013 and any other relevant UK legislation.
 9. These drawings have been produced under the CD 2015 regulations.
 10. The design of this drawing is the property of Sustrans and it is not to be used for any other purpose or in any other form without the prior written consent of Sustrans. Any use of this information in any other form without the prior written consent of Sustrans is prohibited.
 11. To ensure the work is done in accordance with the project drawing, the design should be reviewed.
 12. For further information on drawing and design revisions, see decision log in context. Revisions to be recorded.
 13. This drawing has been produced to be used in colour. For the most accurate reproduction, printing in colour is recommended. In an alternative, where necessary, the use of black and white is recommended.

A. Enfield Design Review | ZH | 00 | 00 | 05/08/2021



Project: NMH Active Travel Improvements

Title: Amersham Avenue Filter General Arrangement

Drawn: Check: Appr: Date: Issue: at A3:
ZH JAG JAG 24/05/2021 1:200

Status: **CONCEPT**
Drawing No: NMH-ATI-C-03-00-01 Revision: A